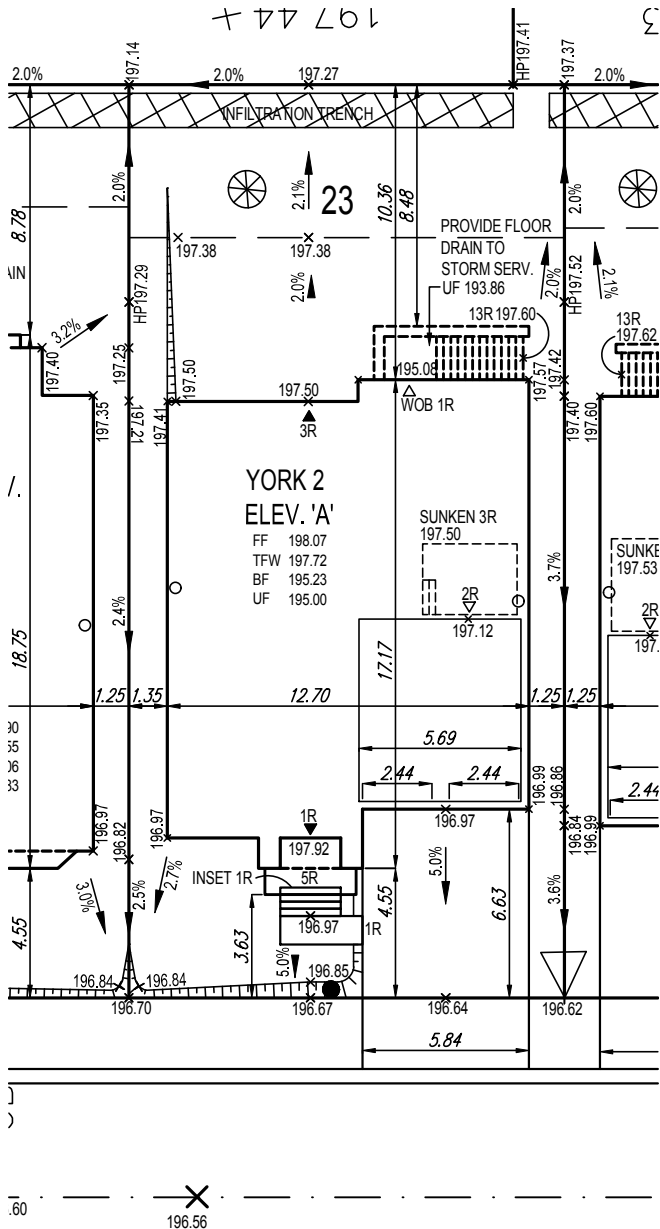


It is the builder's complete responsibility to ensure that all plans submitted for approval fully comply with the Architectural Guidelines and all applicable regulations and requirements including zoning provisions and any provisions in the subdivision agreement. The Control Architect is not responsible in any way for examining or approving site (lotting) plans or working drawings with respect to any zoning or building code or permit matter or that any house can be properly built or located on its lot.

This is to certify that these plans comply with the applicable Architectural Design Guidelines approved by the City of BRAMPTON.



SITE PLAN REVIEW

LOT NO. 23 REGISTERED PLAN 43M-2013

THE PROPOSED LOT GRADING AND DRAINAGE IS APPROVED AS BEING IN CONFORMITY WITH THE OVERALL APPROVED GRADING PLANS FOR THE SUBDIVISION.

THIS REVIEW DOES NOT WARRANT THAT OTHER INFORMATION SHOWN ON THIS DRAWING IS ACCURATE OR COMPLETE. SOLE RESPONSIBILITY FOR THE DESIGN AND DETAILS SHALL REMAIN WITH THE DESIGNER.

NO COMMENT REVIEWED BY SK COMMENTS AS NOTED DATE May 23, 2017

CANDEVCON LIMITED CONSULTING ENGINEERS AND PLANNERS

NOTES

- THE PROPOSED ELEVATIONS AT BUILDINGS AND THE BASIC DIRECTION OF SURFACE DRAINAGE WILL BE SUBJECT TO DETAILED DESIGN WHEN HOUSE TYPES AND PLOT PLANS ARE AVAILABLE.
- REAR TO FRONT DRAINAGE CAN BE ACCOMMODATED ONLY IF THE SEPARATION BETWEEN UNITS IS 2.4M.
- THE HOUSE TYPE AND DRIVEWAY LOCATION SHALL NOT BE CHANGED WITHOUT CITY OF BRAMPTON APPROVAL.
- WHERE POSSIBLE DOWNSPOUTS SHALL DISCHARGE TO THE GROUND SURFACE VIA SPLASH PADS TO THE FRONT OF THE HOUSE.
- A MINIMUM AREA OF 5.0M SHALL BE GRADED AT 2% AT THE REAR OF THE HOUSE.
- A MINIMUM AREA OF 7.5M SHALL BE GRADED AT 2% AT THE REAR OF THE HOUSE FOR REVERSE FRONTAGE LOTS.
- A MINIMUM CLEARANCE OF 0.15M MUST BE PROVIDED BETWEEN THE BRICK LINE AND THE FINAL TOP OF GROUND ELEVATION AT THE HOUSE.
- A MINIMUM CLEARANCE OF 0.20M MUST BE PROVIDED BETWEEN THE SIDING AND THE FINAL TOP OF GROUND ELEVATION AT THE HOUSE.
- ALL LOT GRADING MUST COMPLY WITH CITY OF BRAMPTON STANDARDS 420 AND 421 UNLESS OTHERWISE NOTED.
- DRIVEWAYS SHALL BE GRADED AT MINIMUM OF 2% AND A MAXIMUM OF 8% SLOPE.
- A 0.6M SEPARATION SHALL BE PROVIDED BETWEEN DRIVEWAYS.
- ALL AREAS BEYOND THE PLAN OF SUBDIVISION, WHICH ARE DISTURBED DURING CONSTRUCTION, SHALL BE RESTORED TO THE SATISFACTION OF THE CITY OF BRAMPTON.
- ALL OPEN SPACE BLOCKS AND VALLEY LANDS SHALL BE MAINTAINED FREE OF GARBAGE AND CONSTRUCTION DEBRIS BY THE DEVELOPER UNTIL ASSUMPTION OF THE SUBDIVISION BY THE CITY OF BRAMPTON.

DEGREY DRIVE

SAN INV = 193.87
STM INV = 194.07

PROVIDE POURED IN PLACE CONC. STEPS
ALL STEPS SHALL BE POURED -IN-PLACE
W/ MASONRY VENEER ON THE SIDES

1 RISER
GENERAL
NOTES

THE CONTRACTOR SHALL CHECK AND VERIFY ALL DIMENSIONS AND CONDITIONS ON SITE BEFORE PROCEEDING WITH CONSTRUCTION. ANY DISCREPANCIES SHALL BE REPORTED TO 1 RISER DESIGNS PRIOR TO COMMENCEMENT OF WORK.

1 RISER DESIGNS IS NOT RESPONSIBLE FOR THE ACCURACY OF SURVEY, STRUCTURAL OR ENGINEERING INFORMATION SHOWN ON THESE DRAWINGS OR FOR CONSTRUCTION STARTED PRIOR TO THE ISSUANCE OF A BUILDING PERMIT. REFER TO THE APPROPRIATE ENGINEERING DRAWINGS BEFORE PROCEEDING WITH WORK.

AS CONSTRUCTED INVERTS MUST BE VERIFIED PRIOR TO POURING FOOTINGS.

1 RISER DESIGNS HAS NOT BEEN RETAINED TO CARRY OUT GENERAL REVIEW OF THE WORK AND ASSUMES NO RESPONSIBILITY FOR THE FAILURE OF THE CONTRACTOR OR SUB CONTRACTOR TO CARRY OUT WORK IN ACCORDANCE WITH THE CONTRACT DOCUMENTS.

THIS DRAWING IS AN INSTRUMENT OF SERVICE, IS PROVIDED BY AND IS THE PROPERTY OF 1 RISER DESIGNS. THIS DRAWING IS NOT TO BE SCALED.

	SINGLE SERVICE CONNECTION		VALVE AND CHAMBER		PROP. SWALE		AIR-CONDITIONING UNIT	FF	FINISHED FLOOR ELEVATION
	DOUBLE SERVICE CONNECTION		STREET LIGHT		PROPOSED 3:1 SLOPE		TELECOM. JUNCTION BOX	TW	TOP OF FOUNDATION WALL
	WATER SERVICE CONNECTION		VALVE AND BOX		ENGINEERED FILL LOT		SUMP PUMP	FS	FINISHED BASEMENT SLAB
	HYDRO CONNECTION		SANITARY MANHOLE		TRANSFORMER		EXTERIOR DOOR LOCATION	UF	UNDERSIDE OF FOOTING
	DOUBLE CATCH BASIN		STORM MANHOLE		CABLE TV PEDESTAL		EXTERIOR DOOR LOCATION	UF-R	UNDERSIDE OF FOOTING @ REAR
	CATCH BASIN		COMMUNITY MAIL BOX		BELL PEDESTAL		GRADE PERMITTING	REV.	REVERSE PLAN
	FIRE HYDRANT		DOWNSPOUT LOCATION		HYDRO METER		WOD	WOB	WALK-OUT BASEMENT
					GAS METER		PROPOSED GRADE	WOD	WALK-OUT DECK
							EXISTING GRADE	x 100.00	
							EXISTING GRADE	(100.00)	

1 RISER DESIGNS
20 RIVERMEDE ROAD, UNIT 101
CONCORD, ONTARIO, L4K 3N3
PHONE: (905) 669-2111
FAX: 1 (866) 602-1163
www.1Riser.com

#	Description	Date	By
1	ISSUED FOR PRELIMINARY REVIEW	APR. 26.17	MS
2	ISSUED FOR FINAL APPROVAL	MAY. 18.17	MS

LOT GRADING PLAN
65 DEGREY DRIVE

The undersigned has reviewed and takes responsibility for the design, and has the qualifications and meets the requirements set out in the Ontario Building Code to design the work shown on the attached documents

QUALIFICATION INFORMATION
REQUIRED UNLESS DESIGN IS EXEMPT UNDER DIVISION 3.2.4 OF THE BUILDING CODE
IAN ROBERTSON 27816
REGISTRATION INFORMATION
REQUIRED UNLESS DESIGN IS EXEMPT UNDER DIVISION 3.2.4 OF THE BUILDING CODE
1 RISER DESIGNS Inc. 32072

Client: GREENYORK HOMES
Project: DEGREY DRIVE, CITY OF BRAMPTON

Plan #: 43M-2013
Drawn by: MT
Scale: 1:250
Project #: 17-08

Lot #: LOT 23