COMPLETE ROOF (INCLUDING REAR) CONNECTED TO FRONT DOWNSPOUT AND CONNECTED TO RDC SERVICE CONNECTION. HALF ROOF CONNECTED TO FRONT DOWNSPOUT AND DISCHARGE VIA SPASH PAD. ROOF DISCHARGE/OVERLAND FLOW TO BE DIRECETED TO FRONT OF THE LOT

HALF ROOF CONNECTED TO FRONT DOWNSPOUT AND CONNECTED TO RDC SERVICE CONNECTION. HALF ROOF CONNECTED TO REAR DOWNSPOUT AND

(RR) CONNECTED TO INFILTRATION TRENCH.

1.1 - ROOF DRAINS TO BE CONNECTED AT THE FRONT TO RDC SERVICE
CONNECTION FOR FOOF CONFIGURATIONS RC, RF, & RR
(REFER TO SCS DWG. 906 DETAIL B)

(REFER TO SCS DWG, 906 DETAIL B)

1.2 - IF ROOF CONRIGURATION IS RF OR RC, FRONT ROOF DRAINS TO BE CONNECTED TO FRONT DOWNSPOUT & CONNECTED TO RDC SERVICE CONNECTION. (REFER TO SCS DWG, 906 DETAIL B)

1.3 - IF ROOF CONRIGURATION IS RR, REAR ROOF DRAINS TO BE CONNECTED TO REAR ROOF DOWNSPOUT AND CONNECTED TO INFILTRATION TRENCH (REFER TO SCS DWG, 906 DETAIL A)

1.4 - THE CONTRACTOR SHALL CHECK AND VERIFY ALL GIVEN GRADE ELEVATIONS PRIOR TO COMMENCEMENT OF CONSTRUCTION. FOOTINGS TO BEAR ON NATURAL UNDISTURBED SOIL OR ROCK AND TO BE A MINIMUM OF 1.22m BELOW FINISHED GRADE.

1.22III DELOW FINDRED GRADEY 1.5 - ALL FRONT AND REAR YARDS SHALL BE GRADED AT A 2%-5% GRADE WITHIN 6.0m OF THE DWELLING UNIT. 1.6 - MAXIMUM DRIVEWAY SLOPE SHALL BE 8%.

1.7 - THE MAXIMUM, ALLOWABLE SLOPE IS 3:1 (HORIZONTAL AND VERTICAL) WITH A MAXIMUM ELEVATION DIFFERENCE OF 600mm.

1.8 - DRIVEWAYS TO BE SET BACK A MINIMUM OF 1.0m, FROM ABOVE GROUND

1.8 - DHIVEWAYS 10 BE SET BACK A MINIMUM OF 1.0M, FHOM ABOVE GH SERVICES OR OTHER OBSTRUCTION.

1.9 - LOT HIGH POINT (HP) TO BE 2.0M UPSTREAM OF DOWNSPOUTS

1.10 - ROOF LEADER EMERGENCY OVERFLOW TO DISCHARGE VIA

SPLASH PAD. (REFER TO SCS DWG. 306 DETAIL A FOR ROOF

CONFIGURATION RR AND DETAIL B FOR ROOF CONFIGURATION RC & RF) I.11 - INFILTRATION TRENCHES NOT TO CROSS BETWEEN LOT LINES. REFER TO SCS DWG. 906 DETAIL A)

1.12 - IF ROOF CONFIGURATION IS RR, REAR ROOF DOWNSPOUTS CONNECTED TO 100mm0 CAP, REMOVE CAP AND CONNECT TO REAR LOT INFILTRATION TRENCH, BUILD THE REAR YADR ROOF LEADER CONNECTION TO THE CAP AT THE TRENCHES (TYP.) REFER TO SCS DWG. 906

1.13 - BUILDER TO REFER TO SCS DWG. 906 DETAILS A & B FOR DETAILS ON THE INFILTRATION TRENCH.

## LOT 48 (RESITE)

WE HAVE REVIEWED THE SITE AND GRADING PLAN FOR THE PROPOSED BUILDING TO BE CONSTRUCTED, AND HEREBY

1. The proposed grading and appurtenant drainage works comply

with sound engineering principles.

2. The proposed grading is in conformity with the grading plan approval for this subdivision and will not adversely affect adjacent lands.

 The proposed building is compatible with the proposed grading.
 The proposed water service curb stop is to be located in the grassed portion of the front yard.

The driveway conforms with the City of Vaughan By-Law 1-88 as amended and is a minimum 1.0 metre clear of all street landscape catch basins.

6. The proposed building is a minimum of 0.6 m side yard setback from a drainage swale.

SCS CONSULTING GROUP LTD.



It is the builder's complete responsibility to ensure that all plans submitted for approval fully comply with the Architectural Guidelines and all applicable regulations and requirements including zoning provisions and any provisions in the subdivision agreement. The Control Architect is not responsible in any way for examining or approving site (lotting) plans or working drawings with respect to any zoning or building code or permit matter or that any house can be properly built or located on its lot.

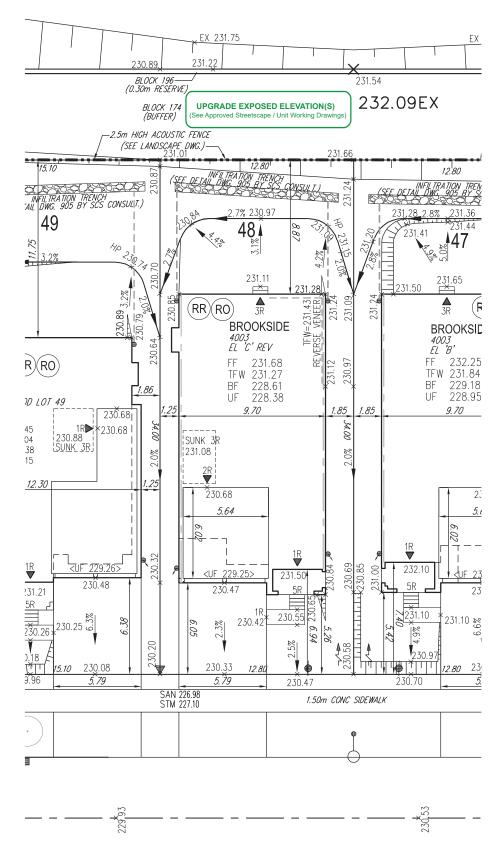
This is to certify that these plans comply with the applicable Architectural Design Guidelines approved by the City of VAUGHAN.

JOHN G. WILLIAMS LTD., ARCHITECT APPROVED BY: DATE: \_ FEB <u>08. 2022</u> is stamp certifies compliance with the applicat Design Guidelines only and bears no further professional responsibility.

COVERAGE CALCULATION						
LOT NO. :	48					
LOT AREA :	435.200000					
BLDG. AREA: (NGL. PORCH)	0.000000					
LOT COVERAGE :	0.00 %					
LANDSCAPE AREA:	0.000000					
ANDSCAPE COV. :	0.00 %					
BUILDING HEIGI	-IT					
MAX BUILDING HEIGHT:	11.000000					
ROM AVERAGE FIN. GRADE@ FRONT OF BUILDING TO MEAN						
ESTABLISHED GRADE:	230,63					
F. TO TOP OF ROOF:	0.000000					
F. TO MEAN OF ROOF:	7.900000					
PROPOSED BLDG. HGT:	8.95 m					
FRONT YARD LANDSCAPE AREA						
FRONT YARD AREA:	68.200000					
ANDSCAPE AREA:	36.800000					
COVERAGE (50% MIN.)	53.96 %					
SOFT LANDSCAPE AREA:	32.900000					
SOFT COVERAGE (60% MIN.):	89.40 %					
REAR YARD LANDSCAPE AREA						
REAR YARD AREA :	113.600000					
SOFT LANDSCAPE AREA :	113.600000					
COVERAGE (60% MIN.):	100 00 %					

22 102235 EP

## TESTON ROAD



## TERRAVISTA CRESCENT

GENERAL NOTES: ATION OF UTILITIES AND OTHER SERVICES. IF MIN. DIMENSIONS ARE NOT MAINTAINED, BUILDER IS TO BELOCATE AT BUILDER'S EXPENSE.

2. BUILDER TO VERIFY ELEV. OF STM. AND SAN. LATERALS IN RELATION TO BASEMENT U/S OF FOOTING ELEVATIONS FOR COMPLIANCE WITH MUNICIPAL STANDARDS PRIOR TO EXCAVATION.

3. APPROVED PERMIT DRAWINGS & CONSTRUCTION NOTES MUST BE REVIEWED AND FOLLOWED IN CONJUNCTION WITH THE SITING AND GRADING PLAN, BUILDER TO VERIFY BUILDING ENVELOPE ON SITE PLAN MATCHES APPROVED PERMIT DRAWINGS & CONSTRUCTION NOTES PRIOR TO POURING CONCRETE. IF THERE ARE ANY DISCREPANCIES, THEY ARE TO BE BROUGHT TO THE ATTENTION OF HUNT DESIGN ASSOCIATES INC.

1. UNLESS NOTED ON BUILDING ENVELOPE OR APPROVED PERMIT DRAWINGS & CONSTRUCTION NOTES, ALL TOP OF FOUNDATION WALLS INCLUDING GARAGE WALLS TO BE CONSISTENT WITH THE ELEVATION PROVIDED FOR TFW ON SITING AND GRADING PLAN. THE EXTERIOR OF THE FOUNDATION WALL TO BE PROVIDED WITH A REDUCTION OF THICKNESS FOR MASONRY VENEER AS REQUIRED.

ı	-		-	-	١,	<i> </i>
ı	REVISED PER CITY ENGINEERING COMMENTS		AW	2022.02.04	1	
ı	REVISED TO 9FT BSMT		AW	2022.01.13	1	<b>\</b>
ı	ISSUED FOR FINAL APPROVAL		AW	2021.01.07	1	
	ISSUED FOR PRELIMINARY APPROVAL		AW	2021.12.15	乚	,
	SANITARY LINE DOWNSPOUTS	SUMP	PUMP AN	D SURFACE	FF	FINISHED FLOOR
_	STORM WATER LINE — - — WINDOWS PERMITTED	DISCHARGE LOCATION  UPGRADE ELEVATION		CATION	TFW BF	TOP OF FOUNDATION WALL BASEMENT FLOOR
	WATERLINE 45 MINUTE FIRE RATED WALL			VATION	UF BF	UNDERSIDE OF FOOTING
	HYDRO LINE SIDEYARD DISTANCE IS LESS				WOD	WALKOUT DECK





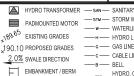


City of Vaughan

**GRADING APPROVED BY** 

**Jason Pham** 

**February 11 2022** 



— G— GAS LINE ---- CABLE LINE

THAN 1.2m TO LOT LINE. (NO WINDOWS PERMITTED) EXTERIOR DOOR LOCATION EXTERIOR DOOR LOCATION IF GRADE PERMITS

CHAIN LINK FENCE FENCE AND GATE PRIVACY FENCE

ACOUSTIC FENCE

WOD WALKOUT BASEMENT
MOD MODIFIED
REV REVERSED

## SITING AND GRADING PLAN

HUNT DESIGN ASSOCIATES INC

QUALIFICATION INFORMATION Allan Whiting 23177 NAME REGISTRATION INFORMATION

19695



TERRAVISTA CRESCENT **GOLDPARK HOMES - 221081** PINE VALLEY PH2 - VAUGHAN, ON

AW AW 1:250 221081WSP01 8966 Woodbine Ave, Markham, ON L3R 0J7 T 905.737.5133 F 905.737.7326 Lot / Page Nun 48