COMPLETE ROOF (INCLUDING REAR) CONNECTED TO FRONT DOWNSPOUT AND CONNECTED TO RDC SERVICE CONNECTION. HALF ROOF CONNECTED TO FRONT DOWNSPOUT AND CONNECTED TO RDC SERVICE CONNECTION. HALF ROOF CONNECTED TO REAR DOWNSPOUT AND CONNECTED TO INFILTRATION TRENCH.

CONNECTED TO INFILITATION TRENCH.

1.1 - ROOF DRAINS TO BE CONNECTED AT THE FRONT TO RDC SERVICE
CONNECTION FOR ROOF CONFIGURATION RC, RF, & RR
(REFER TO SCS DWG. 906 DETAIL B)

1.2 - IF ROOF CONFIGURATION IS RF OR RC, FRONT ROOF DRAINS TO BE
CONNECTED TO FRONT DOWNSPOUT & CONNECTED TO RDC SERVICE
CONNECTED TO FRONT DOWNSPOUT & CONNECTED TO RDC SERVICE
CONNECTION. (REFER TO SCS DWG. 906 DETAIL B)

1.3 - IF ROOF CONFIGURATION IS RR, REAR ROOF DRAINS TO BE CONNECTED
TO REAR ROOF DOWNSPOUT AND CONNECTED TO INFILTRATION TRENCH
(REFER TO SCS DWG. 906 DETAIL A)

1.4 - THE CONTRACTOR SHALL CHECK AND VERIFY ALL GIVEN GRADE
ELEVATIONS PRIOR TO COMMENCEMENT OF CONSTRUCTION, FOOTINGS TO
BEAR ON NATURAL UNDISTURBED SOIL OR ROCK AND TO BE A MINIMUM OF
1.22m BELOW FINISHED GRADE.

1.5 - AALL FRONT AND REAR "XARDS SHALL BE GRADED AT A 2%-5% GRADE
WITHIN 6.0m OF THE DWELLING UNIT.

1.6 - MAXIMUM DRIVEWAY SLOPE SHALL BE 8%.

1.7 - THE MAXIMUM, ALLOWABLE SLOPE IS 3:1 (HORIZONTAL AND VERTICAL)

1.6 - MAXIMUM DRIVEWAY SLOPE SHALL BE 8%.

1.7 - THE MAXIMUM, ALLOWABLE SLOPE IS 3:1 (HORIZONTAL AND VERTICAL)

WITH A MAXIMUM ELEVATION DIFFERENCE OF 600mm.

1.8 - DRIVEWAYS TO BE SET BACK A MINIMUM OF 1.0m, FROM ABOVE GROUND

SERVICES OR OTHER OBSTRUCTION.

1.9 - LOT HIGH POINT (HP) TO BE 2.0m UPSTREAM OF DOWNSPOUTS

1.10 - ROOF LEADER EMERGENCY OVERFLOW TO DISCHARGE VIA

SPLASH PAD. (REFER TO SCS DWG. 906 DETAIL A FOR ROOF

CONFIGURATION RR AND DETAIL B FOR ROOF CONFIGURATION RC & RF)

111 - INIBIT BATTON ITEBRICHES NOT TO CROSS BETWEEN IT JUSTES 1.11 - INFILTRATION TRENCHES NOT TO CROSS BETWEEN LOT LINES. (REFER TO SCS DWG, 906 DETAIL A)

(REFER TO SCS DWG. 906 DETAIL A)
1.12 - IF ROOF CONFIGURATION IS RR, REAR ROOF DOWNSPOUTS CONNECTED
TO 100mm@ CAP. REMOVE CAP AND CONNECT TO REAR LOT INFILTRATION
TRENCH. BUILDER IS RESPONSIBLE TO BUILD THE REAR YARD ROOF LEADER
CONNECTION TO THE CAP AT THE TRENCHES (TYP.) REFER TO SCS DWG. 906
DETAIL A.
1.13 - BUILDER TO REFER TO SCS DWG. 906 DETAILS A & B
FOR DETAILS ON THE INFILTRATION TRENCH.

WE HAVE REVIEWED THE SITE AND GRADING PLAN FOR THE PROPOSED BUILDING TO BE CONSTRUCTED, AND HEREBY

The proposed grading and appurtenant drainage works comply with sound englineering principles.
 The proposed grading is in conformity with the grading plan approval for this subdivision and will not adversely affect

3. The proposed building is compatible with the proposed grading.

4. The proposed water service curb stop is to be located in the

grassed portion of the front yard. The driveway conforms with the City of Vaughan By-Law 1-88 as amended and is a minimum 1.0 metre clear of all street

landscape catch basins. 6. The proposed building is a minimum of 0.6 m side yard setback from a drainage swale.

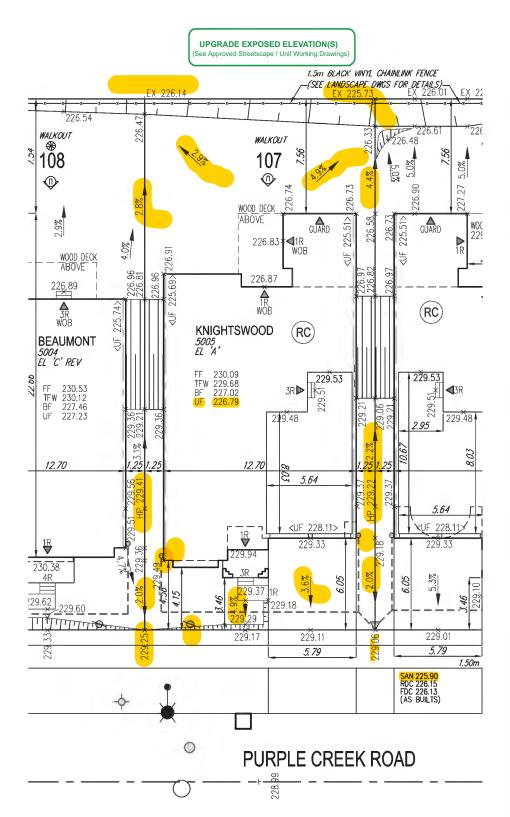
SCS CONSULTING GROUP LTD.

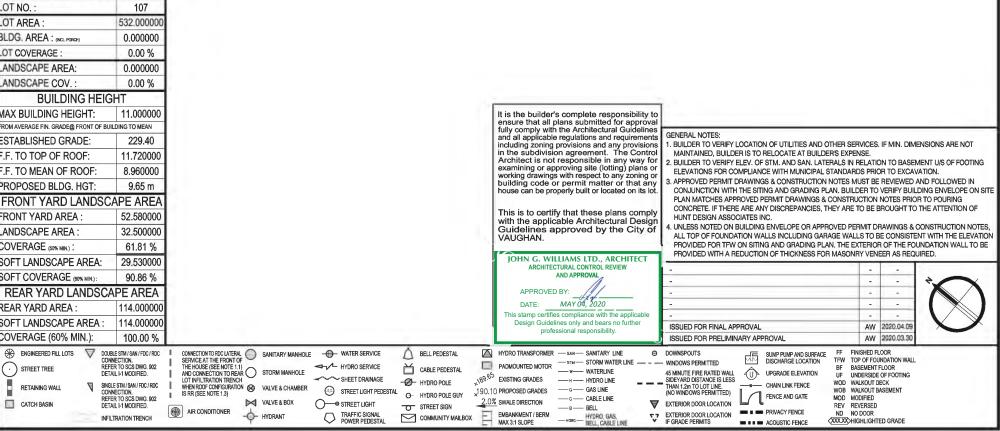
COVERAGE CALCULATION



EVELOPMENT ENGINEERING DEPARTMEN DATE RECEVED: May 13 , 2020 APPROVED By Nino Perez at 4:36 pm, May 27, 2020

## 20-364 EP





SITING AND GRADING PLAN

THE UNDERSIGNED HAS REVIEWED AND TAKES RESPONSIBILITY FOR THIS
DESIGN AND HAS THE QUALIFICATIONS AND MEETS THE REQUIREMENTS SET OUT IN THE ONTARIO BUILDING CODE TO BE A DESIGNER. QUALIFICATION INFORMATION

Allan Whiting REGISTRATION INFORMATION HUNT DESIGN ASSOCIATES INC 19695



**GOLDPARK HOMES - 217020** PINE VALLEY, VAUGHAN ONT.

OF OF 1:250 217020WSP01 8966 Woodbine Ave, Markham, ON L3R 0J7 T 905.737.5133 F 905.737.7326

PURPLE CREEK ROAD

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