

- RC

COMPLETE ROOF (INCLUDING REAR) CONNECTED TO FRONT DOWNSPOUT AND CONNECTED TO RDC SERVICE CONNECTION.
- RF

HALF ROOF CONNECTED TO FRONT DOWNSPOUT AND CONNECTED TO RDC SERVICE CONNECTION.
- RR

HALF ROOF CONNECTED TO REAR DOWNSPOUT AND CONNECTED TO INFILTRATION TRENCH.

- 1.1 - ROOF DRAINS TO BE CONNECTED AT THE FRONT TO RDC SERVICE CONNECTION FOR ROOF CONFIGURATIONS RC, RF, & RR (REFER TO SCS DWG. 906 DETAIL B)
- 1.2 - IF ROOF CONFIGURATION IS RF OR RC, FRONT ROOF DRAINS TO BE CONNECTED TO FRONT DOWNSPOUT & CONNECTED TO RDC SERVICE CONNECTION. (REFER TO SCS DWG. 906 DETAIL B)
- 1.3 - IF ROOF CONFIGURATION IS RR, REAR ROOF DRAINS TO BE CONNECTED TO REAR ROOF DOWNSPOUT AND CONNECTED TO INFILTRATION TRENCH (REFER TO SCS DWG. 906 DETAIL A)
- 1.4 - THE CONTRACTOR SHALL CHECK AND VERIFY ALL GIVEN GRADE ELEVATIONS PRIOR TO COMMENCEMENT OF CONSTRUCTION. FOOTINGS TO BEAR ON NATURAL UNDISTURBED SOIL OR ROCK AND TO BE A MINIMUM OF 1.22m BELOW FINISHED GRADE.
- 1.5 - ALL FRONT AND REAR YARDS SHALL BE GRADED AT A 2%-5% GRADE WITHIN 6.0m OF THE DWELLING UNIT.
- 1.6 - MAXIMUM DRIVEWAY SLOPE SHALL BE 8%.
- 1.7 - THE MAXIMUM, ALLOWABLE SLOPE IS 3:1 (HORIZONTAL AND VERTICAL) WITH A MAXIMUM ELEVATION DIFFERENCE OF 600mm.
- 1.8 - DRIVEWAYS TO BE SET BACK A MINIMUM OF 1.0m, FROM ABOVE GROUND SERVICES OR OTHER OBSTRUCTION.
- 1.9 - LOT HIGH POINT (HP) TO BE 2.0m UPSTREAM OF DOWNSPOUTS
- 1.10 - ROOF LEADER EMERGENCY OVERFLOW TO DISCHARGE VIA SPLASH PAD. (REFER TO SCS DWG. 906 DETAIL A FOR ROOF CONFIGURATION RR AND DETAIL B FOR ROOF CONFIGURATION RC & RF)
- 1.11 - INFILTRATION TRENCHES NOT TO CROSS BETWEEN LOT LINES. (REFER TO SCS DWG. 906 DETAIL A)
- 1.12 - IF ROOF CONFIGURATION IS RR, REAR ROOF DOWNSPOUTS CONNECTED TO 100mmØ CAP. REMOVE CAP AND CONNECT TO REAR LOT INFILTRATION TRENCH. BUILDER IS RESPONSIBLE TO BUILD THE REAR YARD ROOF LEADER CONNECTION TO THE CAP AT THE TRENCHES (TYP.) REFER TO SCS DWG. 906 DETAIL A.
- 1.13 - BUILDER TO REFER TO SCS DWG. 906 DETAILS A & B FOR DETAILS ON THE INFILTRATION TRENCH.

LOT 112

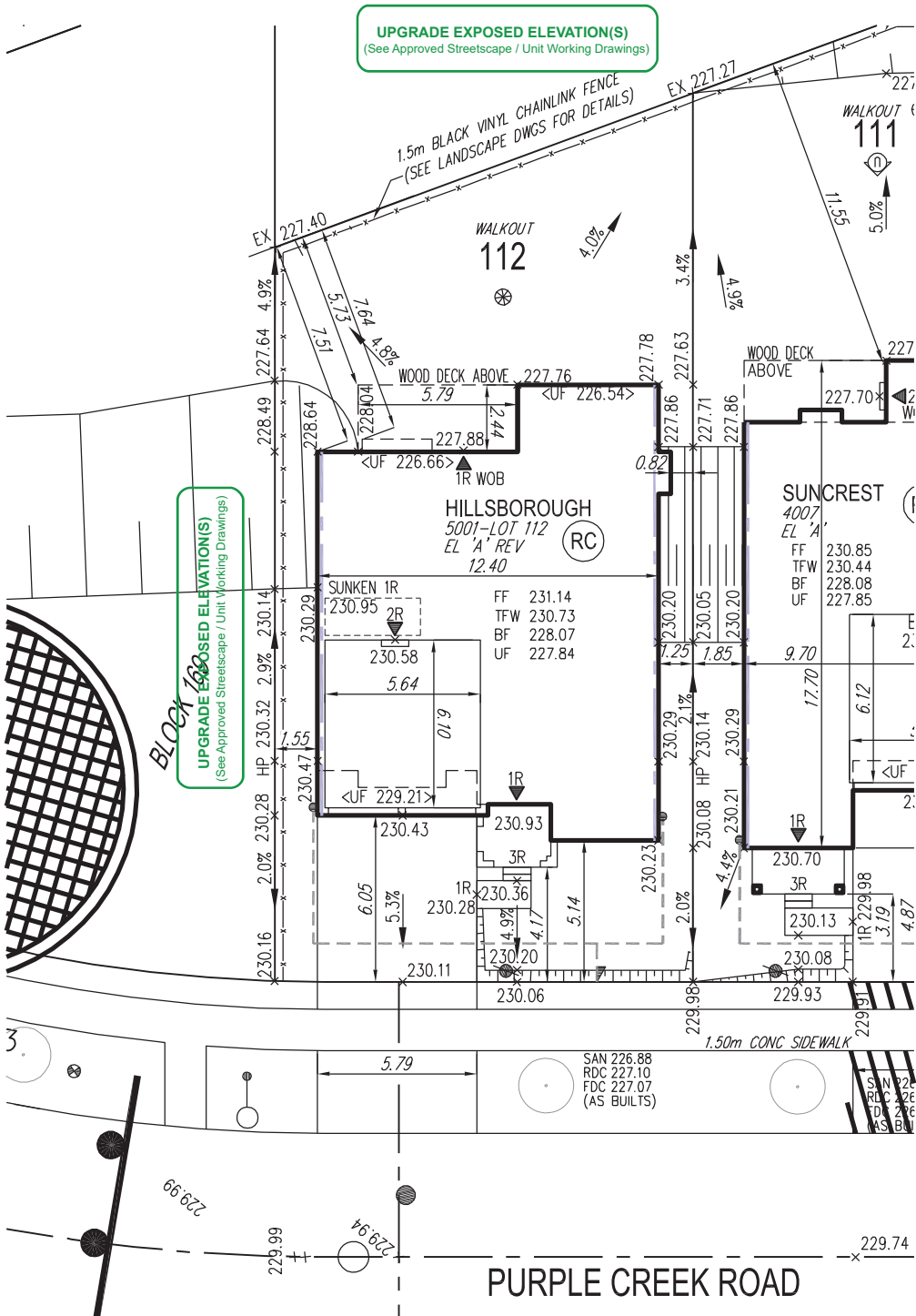
WE HAVE REVIEWED THE SITE AND GRADING PLAN FOR THE PROPOSED BUILDING TO BE CONSTRUCTED, AND HEREBY CERTIFY THAT:

- The proposed grading and appurtenant drainage works comply with sound engineering principles.
- The proposed grading is in conformity with the grading plan approval for this subdivision and will not adversely affect adjacent lands.
- The proposed building is compatible with the proposed grading.
- The proposed water service curb stop is to be located in the grassed portion of the front yard.
- The driveway conforms with the City of Vaughan By-Law 1-88 as amended and is a minimum 1.0 metre clear of all street landscape catch basins.
- The proposed building is a minimum of 0.6 m side yard setback from a drainage swale.

SCS CONSULTING GROUP LTD.



Date: May 4, 2020 Reviewed By: M.R.C.



AIR CONDITIONER ROUGH IN REQUIRED

COVERAGE CALCULATION	
LOT NO. :	112
LOT AREA :	448.210000
BLDG. AREA : (INCL PORCH)	0.000000
LOT COVERAGE :	0.00 %
LANDSCAPE AREA:	0.000000
LANDSCAPE COV. :	0.00 %
BUILDING HEIGHT	
MAX BUILDING HEIGHT:	11.000000
FROM AVERAGE FIN. GRADE@ FRONT OF BUILDING TO MEAN	
ESTABLISHED GRADE:	230.43
F.F. TO TOP OF ROOF:	0.000000
F.F. TO MEAN OF ROOF:	8.360000
PROPOSED BLDG. HGT:	9.07 m
FRONT YARD LANDSCAPE AREA	
FRONT YARD AREA :	63.370000
LANDSCAPE AREA :	39.220000
COVERAGE (50% MIN.):	61.89 %
SOFT LANDSCAPE AREA:	36.270000
SOFT COVERAGE (60% MIN.):	92.48 %
REAR YARD LANDSCAPE AREA	
REAR YARD AREA :	118.400000
SOFT LANDSCAPE AREA :	118.400000
COVERAGE (60% MIN.):	100.00 %

	ENGINEERED FILL LOTS		DOUBLE STM / SAN / FDC / RDC CONNECTION. REFER TO SCS DWG. 902 DETAIL H1 MODIFIED.		CONNECTION TO RDC LATERAL SERVICE AT THE FRONT OF THE HOUSE (SEE NOTE 1.1) AND CONNECTION TO REAR LOT INFILTRATION TRENCH WHEN ROOF CONFIGURATION IS RR (SEE NOTE 1.3)		SANITARY MANHOLE		WATER SERVICE		BELL PEDESTAL		PADMOUNTED MOTOR		EXISTING GRADES		SWALE DIRECTION		EMBANKMENT / BERM MAX 3:1 SLOPE		SANITARY LINE		STORM WATER LINE		WATERLINE		HYDRO LINE		GAS LINE		CABLE LINE		BELL		HYDRO, GAS, BELL, CABLE LINE		DOWNSPOUTS		WINDOWS PERMITTED		45 MINUTE FIRE RATED WALL SIDEYARD DISTANCE IS LESS THAN 1.2m TO LOT LINE. (NO WINDOWS PERMITTED)		EXTERIOR DOOR LOCATION		EXTERIOR DOOR LOCATION IF GRADE PERMITS		SUMP PUMP AND SURFACE DISCHARGE LOCATION		UPGRADE ELEVATION		CHAIN LINK FENCE		FENCE AND GATE		PRIVACY FENCE		ACOUSTIC FENCE		FINISHED FLOOR		TOP OF FOUNDATION WALL		BASEMENT FLOOR		UNDERSIDE OF FOOTING		WALKOUT DECK		WALKOUT BASEMENT		MODIFIED		REVERSED		NO DOOR		HIGHLIGHTED GRADE
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Title

SITING AND GRADING PLAN

THE UNDERSIGNED HAS REVIEWED AND TAKES RESPONSIBILITY FOR THIS DESIGN AND HAS THE QUALIFICATIONS AND MEETS THE REQUIREMENTS SET OUT IN THE ONTARIO BUILDING CODE TO BE A DESIGNER.

QUALIFICATION INFORMATION

Allan Whiting
NAME
SIGNATURE
23177
BCIN

REGISTRATION INFORMATION

HUNT DESIGN ASSOCIATES INC. 19695

HUNT DESIGN ASSOCIATES INC.

www.huntdesign.ca

Plan No. 112
Lot 112
Street Name PURPLE CREEK ROAD

GOLDPARK HOMES - 217020
PINE VALLEY, VAUGHAN ONT.

Drawn By OF
Checked By OF
Scale 1:250
File Number 217020WSP01
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