

- (RC) COMPLETE ROOF (INCLUDING REAR) CONNECTED TO FRONT DOWNSPOUT AND CONNECTED TO RDC SERVICE CONNECTION.
- (RF) HALF ROOF CONNECTED TO FRONT DOWNSPOUT AND CONNECTED TO RDC SERVICE CONNECTION.
- (RR) HALF ROOF CONNECTED TO REAR DOWNSPOUT AND CONNECTED TO INFILTRATION TRENCH.
- 1.1 - ROOF DRAINS TO BE CONNECTED AT THE FRONT TO RDC SERVICE CONNECTION FOR ROOF CONFIGURATIONS RC, RF, & RR (REFER TO SCS DWG. 906 DETAIL B)
- 1.2 - IF ROOF CONFIGURATION IS RF OR RC, FRONT ROOF DRAINS TO BE CONNECTED TO FRONT DOWNSPOUT & CONNECTED TO RDC SERVICE CONNECTION. REFER TO SCS DWG. 906 DETAIL B)
- 1.3 - IF ROOF CONFIGURATION IS RR, REAR ROOF DRAINS TO BE CONNECTED TO REAR DOWNSPOUT AND CONNECTED TO INFILTRATION TRENCH (REFER TO SCS DWG. 906 DETAIL A)
- 1.4 - THE CONTRACTOR SHALL CHECK AND VERIFY ALL GIVEN GRADE ELEVATIONS PRIOR TO COMMENCEMENT OF CONSTRUCTION. FOOTINGS TO BEAR ON NATURAL UNDISTURBED SOIL OR ROCK AND TO BE A MINIMUM OF 1.2m BELOW FINISHED GRADE.
- 1.5 - ALL FRONT AND REAR YARDS SHALL BE GRADED AT A 2%-5% GRADE WITHIN 6.0m OF THE DWELLING UNIT.
- 1.6 - MAXIMUM DRIVEWAY SLOPE SHALL BE 8%.
- 1.7 - THE MAXIMUM ALLOWABLE SLOPE IS 3:1 (HORIZONTAL AND VERTICAL) WITH A MAXIMUM ELEVATION DIFFERENCE OF 600mm.
- 1.8 - DRIVEWAYS TO BE SET BACK A MINIMUM OF 1.0m, FROM ABOVE GROUND SERVICES OR OTHER OBSTRUCTION.
- 1.9 - LOT HIGH POINT (H/P) TO BE 2.0m UPSTREAM OF DOWNSPOUTS
- 1.10 - ROOF LEADER EMERGENCY OVERFLOW TO DISCHARGE VIA SPLASH PAD. REFER TO SCS DWG. 906 DETAIL A FOR ROOF CONFIGURATION RR AND DETAIL B FOR ROOF CONFIGURATION RC & RF (REFER TO SCS DWG. 906 DETAIL A)
- 1.11 - INFILTRATION TRENCHES NOT TO CROSS BETWEEN LOT LINES.
- 1.12 - IF ROOF CONFIGURATION IS RR, REAR ROOF DOWNSPOUTS CONNECTED TO 100mmØ CAP, REMOVE CAP AND CONNECT TO REAR LOT INFILTRATION TRENCH. BUILDER IS RESPONSIBLE TO BUILD THE REAR YARD ROOF LEADER CONNECTION TO THE CAP AT THE TRENCHES (TYP.) REFER TO SCS DWG. 906 DETAIL A.
- 1.13 - BUILDER TO REFER TO SCS DWG. 906 DETAILS A & B FOR DETAILS ON THE INFILTRATION TRENCH.

LOT 97

WE HAVE REVIEWED THE SITE AND GRADING PLAN FOR THE PROPOSED BUILDING TO BE CONSTRUCTED, AND HEREBY CERTIFY THAT:

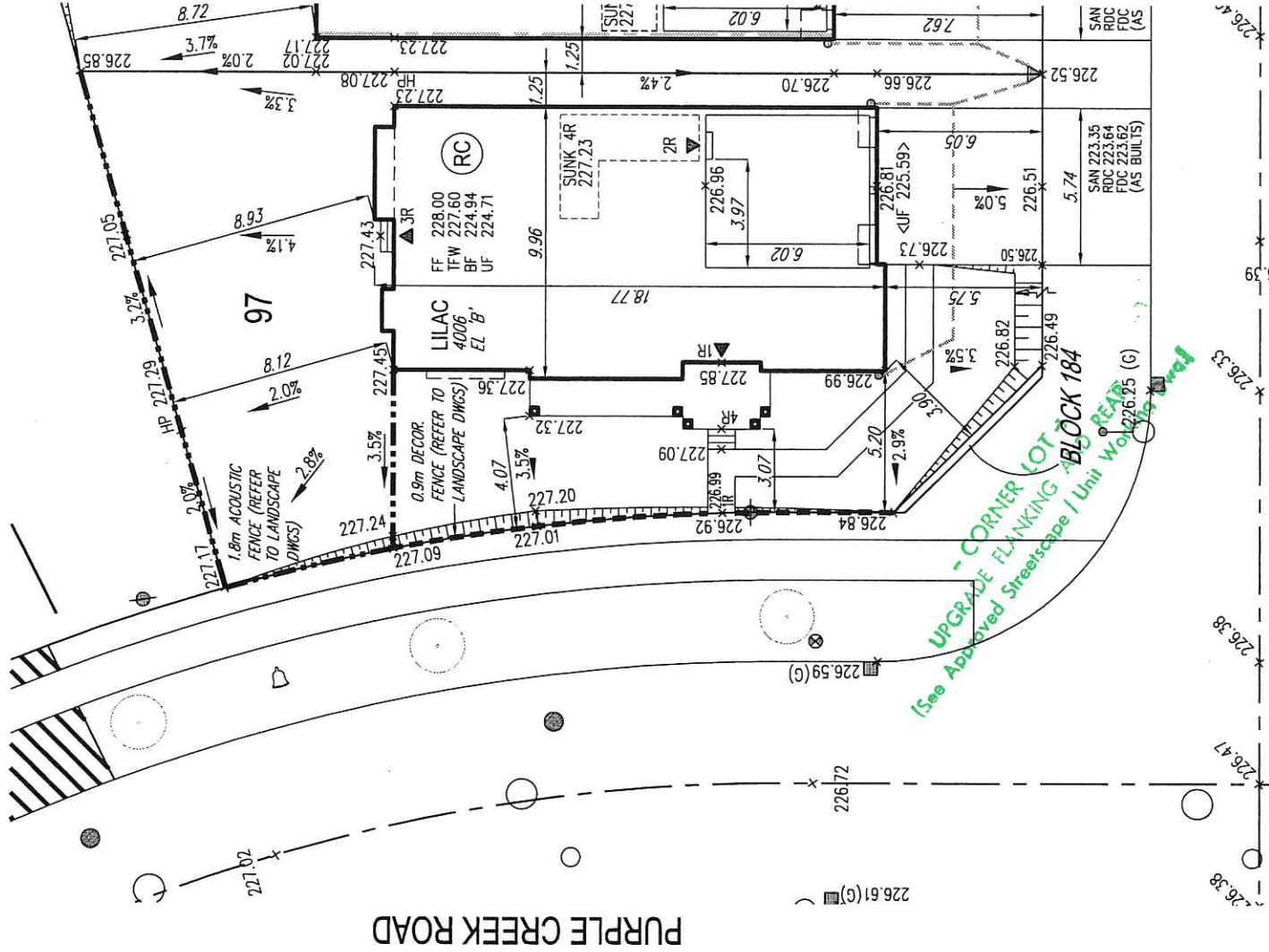
- The proposed grading and appurtenant drainage works comply with sound engineering principles.
- The proposed grading is in conformity with the grading plan approval for this subdivision and will not adversely affect adjacent lands.
- The proposed building is compatible with the proposed grading.
- The proposed water service curb stop is to be located in the grassed portion of the front yard.
- The driveway conforms with the City of Vaughan By-Law 1-88 as amended and is a minimum 1.0 metre clear of all street landscape catch basins.
- The proposed building is a minimum of 0.6 m side yard setback from a drainage swale.

SOS CONSULTING GROUP LTD.

E. RAMIREZ CORTES
100186424
Nov 5/18
PROVINCE OF ONTARIO

Date: Nov 5/18 Reviewed By: E.R.

COVERAGE CALCULATION	
LOT NO.:	97
LOT AREA:	539.94 m ²
BLDG. AREA: (INCL. PORCH)	191.84 m ²
LOT COVERAGE:	35.53 %
LANDSCAPE AREA:	313.37 m ²
LANDSCAPE COV.:	58.04 %
BUILDING HEIGHT	
MAX BUILDING HEIGHT:	11.00 m
FROM AVERAGE FIN. GRADE@ FRONT OF BUILDING TO MEAN	
ESTABLISHED GRADE:	227.24
F.F. TO TOP OF ROOF:	10.51 m
F.F. TO MEAN OF ROOF:	8.33 m
PROPOSED BLDG. HGT:	9.09 m
FRONT YARD LANDSCAPE AREA	
FRONT YARD AREA:	80.00 m ²
LANDSCAPE AREA:	47.02 m ²
COVERAGE (60% MIN.):	58.78 %
SOFT LANDSCAPE AREA:	41.40 m ²
SOFT COVERAGE (60% MIN.):	88.05 %
REAR YARD LANDSCAPE AREA	
REAR YARD AREA:	150.18 m ²
SOFT LANDSCAPE AREA:	150.18 m ²
COVERAGE (60% MIN.):	100.00 %



SOPHIES COURT

AIR CONDITIONER ROUGH
IN REQUIRED

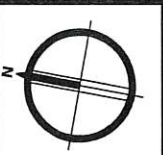
It is the builder's complete responsibility to ensure that all plans submitted for approval fully comply with the Architectural Guidelines and all applicable zoning regulations and provisions including zoning provisions and any provisions in the subdivision agreement. The Control Architect is not responsible in any way for examining or approving site (lotting) plans or working drawings with respect to any zoning or building code or permit matter or that any house can be properly built or located on its lot.

This is to certify that these plans comply with the applicable Architectural Design Guidelines approved by the City of VAUGHAN.

ARCHITECTURAL REVIEW & APPROVAL
NOV 06 2018
John G. Williams Limited, Architect

- GENERAL NOTES:
- BUILDER TO VERIFY LOCATION OF UTILITIES AND OTHER SERVICES. IF MIN. DIMENSIONS ARE NOT MAINTAINED, BUILDER IS TO RELOCATE AT BUILDER'S EXPENSE.
 - BUILDER TO VERIFY ELEV. OF STM. AND SAN. LATERALS IN RELATION TO BASEMENT LUS OF FOOTING ELEVATIONS FOR COMPLIANCE WITH MUNICIPAL STANDARDS PRIOR TO EXCAVATION.
 - APPROVED PERMIT DRAWINGS & CONSTRUCTION NOTES MUST BE REVIEWED AND FOLLOWED IN CONJUNCTION WITH THE SITING AND GRADING PLAN. BUILDER TO VERIFY BUILDING ENVELOPE ON SITE PLAN MATCHES APPROVED PERMIT DRAWINGS & CONSTRUCTION NOTES PRIOR TO POURING CONCRETE. IF THERE ARE ANY DISCREPANCIES, THEY ARE TO BE BROUGHT TO THE ATTENTION OF HUNT DESIGN ASSOCIATES INC.
 - UNLESS NOTED ON BUILDING ENVELOPE OR APPROVED PERMIT DRAWINGS & CONSTRUCTION NOTES, ALL TOP OF FOUNDATION WALLS INCLUDING GARAGE WALLS TO BE CONSISTENT WITH THE ELEVATION PROVIDED FOR TFV ON SITING AND GRADING PLAN. THE EXTERIOR OF THE FOUNDATION WALL TO BE PROVIDED WITH A REDUCTION OF THICKNESS FOR MASONRY VENEER AS REQUIRED.

REVISED AS PER ENGINEER & REISSUED FOR FINAL	OF	2018/11/01
ISSUED FOR PRELIMINARY APPROVAL	OF	2018/10/15



SYMBOL	DESCRIPTION
FINISHED FLOOR	FF
TOP OF FOUNDATION WALL	TFW
UNDERSIDE OF FOOTING	UF
WOOD WALKOUT DECK	WOD
WOOD WALKOUT BASEMENT	WOB
MOD. MODIFIED	MOD
REV. REVERSED	REV
NO DOOR	NO
XXXXX-HIGHLIGHTED GRADE	XXXXX

SYMBOL	DESCRIPTION
DOWNSPOUTS	DOWNSPOUTS
WINDOWS PERMITTED	WINDOWS PERMITTED
45 MINUTE FIRE RATED WALL	45 MINUTE FIRE RATED WALL
SIDEWALK DISTANCE IS LESS THAN 1.2m TO LOT LINE	SIDEWALK DISTANCE IS LESS THAN 1.2m TO LOT LINE
NO WINDOWS PERMITTED	NO WINDOWS PERMITTED
EXTERIOR DOOR LOCATION	EXTERIOR DOOR LOCATION
EXTERIOR DOOR LOCATION IF GRADE PERMITS	EXTERIOR DOOR LOCATION IF GRADE PERMITS
HYDRO TRANSFORMER	HYDRO TRANSFORMER
SAN. STORM WATER LINE	SAN. STORM WATER LINE
WATERLINE	WATERLINE
HYDRO LINE	HYDRO LINE
GAS LINE	GAS LINE
CABLE LINE	CABLE LINE
BELL	BELL
HYDRO GAS	HYDRO GAS
BELL CABLE LINE	BELL CABLE LINE